



Guidelines for Small-Lot Single-Family Residential Developments

May 10, 2005



City of Modesto
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City of Modesto

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Planning Division

Approved by Modesto City Council Resolution Number 2005-253

**MODESTO CITY COUNCIL
RESOLUTION NO. 2005-253**

A RESOLUTION APPROVING THE GUIDELINES FOR SMALL-LOT SINGLE-FAMILY RESIDENTIAL DEVELOPMENTS

WHEREAS, Urban Area General Plan Goal E seeks to provide and maintain attractive residential neighborhoods with a variety of dwelling types and prices affordable to all segments of the population, and

WHEREAS, Urban Area General Plan Goal I.E.5 promotes equal opportunity for all residents to reside in the housing of their choice, and

WHEREAS, Urban Area General Plan Policy II.B.1.a requires sufficient land be made available to support future expansion of the City, and

WHEREAS, Urban Area General Plan Policy III.B.1 recommends residential density of 7.5 dwelling units per gross acre, and

WHEREAS, new residential development in the City is typically occurring at between four and five dwelling units per gross acre, and

WHEREAS, allowing residential development to occur at increased density improves the efficiency of land consumption and improves the sufficiency of the existing land supply for City expansion, and

WHEREAS, Government Code Section 65850, et. seq. permits Cities and Counties to adopt ordinances that regulate the use of buildings, structures, and land, and

WHEREAS, Title 10 of the Modesto Municipal Code establishes regulations for the use of land and buildings and the purpose of Article 17, Planned Development Zone (P-D), is to encourage creative and efficient land uses, encourage mixed or multiple use

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Modesto that it hereby finds and determines as follows:

1. The "Guidelines for Small-Lot Single-Family Residential Developments" is consistent with the General Plan, because it allows the development of dwelling types not currently offered or in short supply; improves efficient use of land and extends the sufficiency of the existing land supply; encourages creative and efficient land uses; permits variations from the density, height, and other standards in the various zones; and provides guidance to the development community for high standards of performance and design.
2. The "Guidelines for Small-Lot Single-Family Residential Developments" is a policy document and therefore is not a project in accordance with Section 15378(b)(2) of CEQA Guidelines.

BE IT FURTHER RESOLVED by the City Council of the City of Modesto that it hereby adopts the "Guidelines for Small-Lot Single-Family Residential Developments," a copy of which is attached hereto as Exhibit A and incorporated herein by reference.

The foregoing resolution was introduced at the regular meeting of the Council of the City of Modesto held on the 10th day of May 2005, by Councilmember Jackman, who moved its adoption, which motion being duly seconded by Councilmember Keating, was upon roll call carried and the resolution adopted by the following vote:

Ayes: Councilmembers: Dunbar, Hawn, Jackman, Keating, Marsh, O'Bryant, Mayor Ridenour

Noes: Councilmembers: None

Absent: Councilmembers: None

Attest: Jean Zahr
JEAN ZAHR, City Clerk

By: Michael D. Milich
MICHAEL D. MILICH, City Attorney

Guidelines for Small Lot Single-Family Residential Development

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Guidelines for Small-Lot Single-Family Residential Developments

PURPOSE

The purpose of the Guidelines for Small-Lot Single-Family Residential Developments is to provide a clear set of design policies to project sponsors such as developers, property owners, architects and designers. These are the primary design issues that the planning staff, City Council and Planning Commission will use to evaluate project proposals. The goal is to expedite the planning review process by clearly stating the City's desires for quality design of small-lot residential projects. Safety, livability, and long-term viability will guide the evaluation of small-lot developments.

Small-lot development meets the needs of people who do not have the time or ability to maintain larger lots and houses. Properly located and with a well-designed street network, higher-density development reduces distances between homes and shopping, which can reduce driving and support increased transit use, and can help meet some of the goals of the General Plan.

OBJECTIVES

The Guidelines are intended to address the following objectives:

- Promote high quality development.
- Create residential neighborhoods that provide interest and are visually pleasing.
- Small-lot single-family projects that feature a variety of lot types, home sizes, housing types, designs, and building materials.
- Small-lot single-family developments that include interconnected, short blocks that diffuse traffic and provide easy, direct routes for pedestrians, bicyclists, and drivers around the neighborhood.
- Small-lot single-family developments that emphasize pedestrian-oriented streetscapes, not dominated by garages, that includes street systems designed for pedestrians and bicyclists as well as for automobile use.
- Small-lot single-family projects that are integrated and compatible with existing neighborhoods adjacent to them.

INTENT

The following design guidelines are to be used to assist developers, project applicants and City staff in producing a quality Planned Development. City staff and Planning Commissioners will use these Guidelines as a framework for evaluating development proposals and for commenting on the design aspects of proposed projects.

The Guidelines will be used to augment and reinforce the Planned Development zone, Modesto Municipal Code Title 10, Article 17. The Small-Lot Guidelines are general and may be interpreted with some flexibility in their application to specific projects. Variations may be considered for projects with special design characteristics during the City's development review process to encourage the highest level of design quality while at the same time providing the flexibility necessary to encourage creativity on the part of project designers. The Guidelines are also intended to ensure that new development is compatible with existing neighborhoods.

APPLICABILITY

The Guidelines for Small-Lot Single Family Residential Developments apply to single-family detached residential development proposals. Small-lot development can only occur in Specific Plan areas and in Planned Development zones; the minimum lot size in the R-1 zone is 5,000 square feet, therefore, these Guidelines would not apply.

There is a minimum practical lot size that will accommodate one detached house and still meet the intent of these Guidelines for small-lot development. Rather than place a limit on lot size, these Guidelines allow the project designer maximum flexibility to develop a quality project that meets the intent of the Guidelines.

Lots will be evaluated in two size ranges: less than 3,000 square feet and between 3,000 and 5,000 square feet (Table 1). Lot frontage will suggest other siting criteria.

PLANNING COMMISSION AND CITY COUNCIL

A Planned Development is subject to review and approval by the Planning Commission and the City Council. Projects are assessed for conformance with the Guidelines by staff prior to consideration by these bodies. The Planning Commission shall hear and make recommendations on Planned Development applications to the City Council. A Planned Development requires final review and approval by the City Council (Title 10, Article 17, Modesto Municipal Code).

DISCRETIONARY DECISION MAKING

Every project is unique and requires a review on a case-by-case basis. This process depends upon the exercise of discretion. While some Guidelines include quantitative standards, some require qualitative interpretation. The City has the latitude to interpret the Guidelines so long as proposed projects meet the Guidelines' intent.

OTHER APPLICABLE REGULATIONS

The Guidelines primarily address architectural and site design elements. In designing projects, designers must also reference other codes, standards and policies in effect, such as the City of Modesto Standard Specifications, Uniform Building/Fire Code, Modesto Urban Area General Plan, etc.

GUIDELINES

I. Small-Lot Single-Family Detached Houses

Conventional definitions of setbacks apply to most housing types, but not to all. Courthomes are a type of housing where houses on individual lots are arranged around a common driveway that takes access from a street. Because these houses have an unusual relationship to each other and to the street, Section II addresses courthomes specifically.

A. Relationship to Existing Neighborhoods

New small-lot residential projects should be integrated with the existing neighborhoods adjacent to them. Designs should avoid the separation caused by high, solid fencing and walls, or blank walls of buildings.

Transitions between existing and new projects should be gradual. The height and mass of new projects should not create abrupt changes from those of existing buildings. Site setbacks should continue the prevailing setback patterns of adjacent buildings.

The perimeter areas of new projects should be planned to avoid disturbing existing adjacent residential uses. The protection of privacy of adjacent residents and minimization of environmental intrusions should be a major consideration in the design of new projects.

Where existing neighborhoods have architectural distinction and/or established functional or landscape patterns, new development should incorporate characteristics of the surroundings so that there is no disruption of the streetscape.

B. Setbacks/Open Space

Setbacks: The front setback establishes a relationship between the house and the surrounding neighborhood. If the house is too close, indoor privacy can be compromised. If the house is too far back, people inside the house cannot observe activity on the street. Additionally, a larger front yard setback leaves a smaller private rear yard. Each group of three adjacent houses should contain at least one house whose front setback to the living area differs from those of its neighbors by a minimum of three feet (Fig. 1).



An example of single-family homes on small lots.



Poor transition: an older single-family ranch house surrounded by much larger two-story Mediterranean-style homes, causing architectural disparity as well as loss of privacy for the older home.

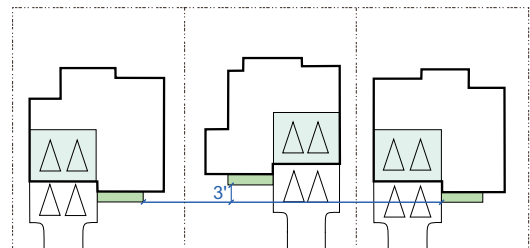


Fig. 1: Varied Front Setbacks

The side setback is primarily utilitarian. However, living areas of the house usually have windows that open into the side yard. Normal activity in the side yard, although limited, tends to be irritating because the noise is concentrated in a small space and can be loud and irregular, such as moving trash cans or using storage sheds.

The rear yard is where most outdoor activity around the house occurs. The rear yard is where people typically expect to have privacy outdoors and is where children play and entertaining happens. These activities are expected and noisy, but the noise can be considered intrusive by neighbors; residents can feel as if their outdoor or indoor privacy is being invaded by rear-yard neighbors. Adequate space is necessary for residents to enjoy their yards while providing a sense of privacy.

Private open space, typically in the rear yard, provides a place for children to play and to entertain friends. It must be large enough to allow these activities while maintaining some sense of privacy on both sides of the fence. The design of private yards is of greater importance than in larger lots, given that most personal and limited outdoor area will be the private yard or courtyard. Model homes should display a variety of fencing and landscape design concepts including porches, patios, walkways, covered trellises, screens, and garden walls. Private open space can occur in the form of a rear yard, patio, balcony, and/or deck. Private open spaces should be contiguous to the units they serve, screened from view, and have usable configurations.

Common open space: Common open space is required for developments of 15 units or greater; the minimum size and dimensions of common open space is prescribed in Table 1. Common open space should be centrally located so that it is a focus for the neighborhood and must be easily viewed from the street and homes for informal surveillance and security (Fig. 2). Common open space must be usable, and only landscaping that enhances its utility is permitted. In addition to lawn area, common open space should contain up to 15% of the open space area dedicated to decorative landscaping. Open space elements include play equipment, and seating and tables in the larger play areas. Tot lots and parks should be designed to facilitate use by a number of different ages or activity groups concurrently,



Common open space should be easily viewed from homes and the street for informal surveillance and security.



Common open space should be centrally located to be shared by the neighborhood.



Fig. 2: Common open space that is centrally located to the development, and easily accessed by residents.

such as for small gatherings and may include small barbeques and ample seating and tables. A reduction in common open space may be considered if the project is immediately adjacent to a public park. Where developments are very near or adjacent to a public park or trail, direct pedestrian access to the public park or trail is required. The requirements set forth for common open space will not alleviate the requirement for CFF and/or CFD fees for park development and maintenance; all current park fees will remain in effect. Common open space landscape design must be approved by a designated representative of the Parks, Recreation and Neighborhoods Department.

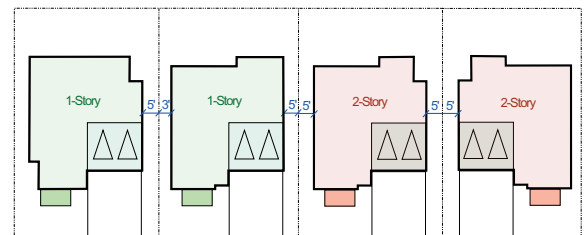
Table 1 displays the various setback and open space provisions for lots ranging in size from 3,000 to 5,000 square feet and those below 3,000 square feet. Variations to the following setbacks may be considered for projects with special design characteristics to achieve the highest level of design quality as noted above:

Table 1: Setbacks and Open Space (in feet, measured from property line)		
Lot Size	<3,000 sq. ft.	3,000 – 5,000 sq. ft.
FRONT SETBACKS (a)		
Living (1 st floor) (vary front setback by 3' as noted above)	12 minimum	12 minimum
Living Area (2 nd floor)	15	15
Porches	10	10
Attached Garage (b) (entry/non-entry side)	20/15	20/15
REAR SETBACKS *		
Living Area	10	15
Attached Garage (no alley/alley access) (c)	10/4	10/4
Detached Garage (no alley/alley access) (c)	5/4	5/4
Patio Covers (d)	7	7
SIDE SETBACKS * (a)(g)(h)(i)		
Living Area first floor (interior side) (e)(f)	5 one side, 3 other side (g) or 10/0	5 one side, 3 other side (g) or 10/0
Living Area second floor (interior side)	5	5
Living Area (corner side) (e)	15	15
Detached Garage (Int. Side)	0	0
Attached/Detached Garages (Corner Side) entry side/non-entry side	20/10	20/10
PRIVATE OPEN SPACE		
Minimum Area (sq. ft.) per unit	300	500
Minimum Dimension	15	15
COMMON OPEN SPACE		
Minimum Area (sq. ft.) per unit (j)	300	250

NOTES:

- a. Dwelling unit and wall/fencing shall be located outside the "clear vision triangle area" at street intersections, per City Standards, Detail No. 711.
- b. Garage setback measured from sidewalk, with roll-up doors.
- c. Garages taking access off an alley are to provide a minimum four-foot setback/apron.
- d. Patio covers open on three sides should not exceed 30 percent of the size of the usable private open space.
- e. Includes attached garages and patio covers.
- f. The intent of "zero-foot" side yard on one side is to encourage a larger or more useful side yard on one side of the house. The required dimensions apply to fenced yard sizes rather than actual building setbacks from the property line. "Zero Lot Line" or similar developments require maintenance access easements.
- g. 10' separation for one- and two-story units side-by-side or two two-story units side-by-side; eight-foot separation for two one-story units side-by-side.
- h. Use of zero-foot side setback on one side is encouraged (other side setback doubled) in order to increase overall private yard area (this does not allow a reduction in open space requirements noted in the table).
- i. Minor architectural projections, such as fireplaces and bay windows, may project into a setback or separation by up to two feet for a length not to exceed ten feet or 20 percent of the building elevation length, with minimum three-foot clearance.
- j. Fragments less than 15' will not be counted toward the common open space area.

* Accessory rental units/"Granny Flats" shall adhere to accessory dwelling unit ordinance (pending completion as of May 2005) Refer to Table 3 (page 5) for setbacks applicable to three-story houses.



The minimum separation between houses is 8' between single-story homes and 10' between 2-story homes or 2-story homes beside 1-story homes.

Fig. 3: Side-Yard Separations (Relationship Between Single-Story and Two-Story Homes)

Table 2 identifies setback standards for key project elements that are adjacent to existing and proposed perimeter streets:

Table 2: Perimeter Setbacks (in feet)				
Project Components	Perimeter Street is:			
	Local Street	Collector Street	Arterial Street	Expressway
Balcony/Deck	20	20	20	30
Residential Bldg.	15	20	35	35
Detached Garage (entry face)	40	40	NA	NA
Detached Garage (other face)	15	15	15	15

C. Lot Variation

Single-family lot patterns should be varied to avoid monotonous streetscapes. This could be accomplished by the following:

Encourage:

- A variety of lot types and/or sizes on blocks with more than five lots. Lot sizes should vary enough to create a perceptible difference.
- Single-story buildings and larger lots on corners.
- Smaller lots surrounding common open space areas.
- Mix of single- and two-story units.

Discourage/Avoid:

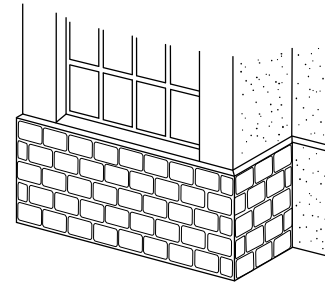
- Streets with more than five consecutive lots of the same width frontage.
- Blocks more than 600 feet long.

D. General Building Design

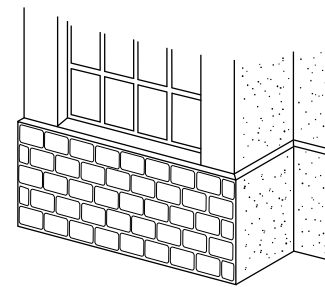
Variation in residences, structures and buildings is achieved through the use of quality materials and detail in design, which lends visual interest, distinctive character and identity to a community. Quality in detail and design contributes not only to the long-term value of a home, but the neighborhood as well.

Encourage:

- Design diversity by providing front elevation variation throughout the plan. To accomplish this, one design should be repeated no more frequently than each fourth house. Veneer treatment where applied should turn corners and avoid exposed edges (Fig. 4)
- Provide four-sided architecture. In addition to the architectural design provided for the front elevation, design side and rear elevations to include treatments (e.g. window frames, shutters, planter boxes, window sills, etc.)



Desirable Treatment



Undesirable Treatment

Fig. 4: Veneer Wrapping



Example of four-sided architecture and design treatments: veneer wrapping, window sills, and decorative bracing along roofline.

- At corner lots, side yard facades should maintain the same architectural design as the front facade.
- Manipulation of building elements and massing to avoid visual monotony with particular emphasis on long streets.
- Vary roof forms and pitches when a project includes five or more homes. Incorporate home designs that rotate ridge lines both parallel and perpendicular to the street and utilize a variety of hips and gables. Other elements which add variety and break up the roof, such as dormers and turrets, are encouraged.
- Roof elements of a two-story building that slope downward toward the side property lines, providing greater light and air between buildings, particularly when the separation between the floors of the two adjoining buildings would be less than 15 feet (Fig. 5).
- All houses along a block should share a common architectural theme, which creates visual continuity.
- Single-story homes distributed evenly throughout the neighborhood to provide for seniors, the disabled, and families who prefer or desire single-story homes. Single-story homes are also encouraged to improve the visual character of neighborhoods and minimize the perceived density of two-story neighborhoods.

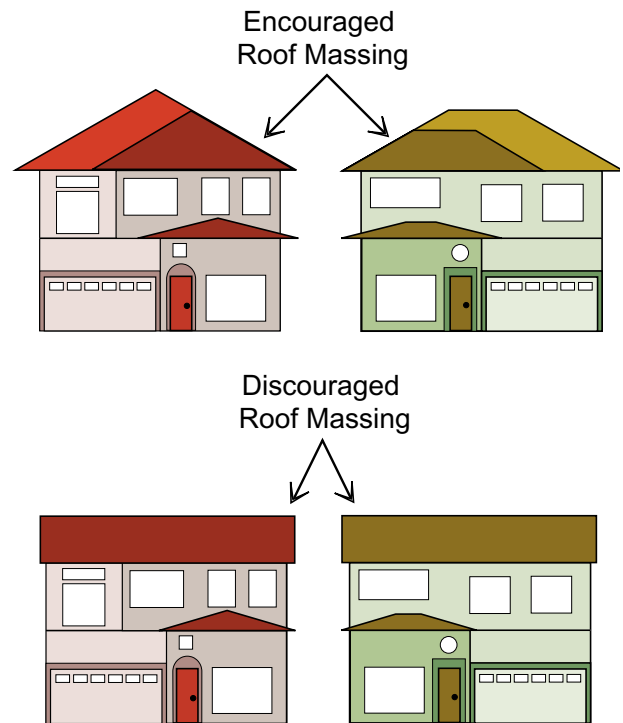


Fig. 5: Roof Massing

Discourage/Avoid:

- Excessive repetition of identical floor plans and elevations throughout a neighborhood or subdivision with little differentiation.
- The use of low quality/grade materials that do not wear well or contribute to a sense of permanence.
- Roof-mounted heating and air conditioning.
- Keyhole entries (primary entrance hidden from view on the side or within deep recess of the building) should be avoided.



Roof-mounted HVAC units are strongly discouraged.

E. Three-Story Houses

If three-story houses are proposed, their number should be limited to a maximum of 20 percent of the subdivision, with placement adjacent to multi-family residential or commercial uses only and strongly discouraged adjacent to established single-family neighborhoods and/or single-story units within a small-lot development. They should provide alley-loaded garages only, avoid monolithic walls in architectural design and massing, have minimum setbacks as prescribed in Table 3, be designed with consideration towards privacy of surrounding homes, and apply private/common open space, landscaping, and streetscape elements as illustrated by these Design Guidelines.

Lot Size	<3000 sq ft	3000-5000 sq ft
Front		
1st Story	12	12
2nd Story	15	15
3rd Story	17	17
Side		
1st and 2nd Stories	5	5
3rd Story	7	7
Rear		
Living Area	12	15

F. Porches, Entries, and Courts

A clear sense of entry and design interest to a home is provided through the inclusion of porches, verandas, porte cocheres, and other architectural elements that contribute to a sense of place and activity.

Encourage:

- Fronts of houses and entries that face the street. Each house should have a clearly identified entry and have active use of windows (i.e. living room, kitchen) facing the street.
- Front porches large enough to accommodate chairs provide an opportunity for increased interaction among neighbors (minimum dimension of 6'x6' plus circulation area).
- Porches that provide weather protection and shade are desired.
- Entries and porches that incorporate railings, short walls, trellises, and roofs to add architectural detail, character, and visual interest to homes.

Discourage/Avoid:

- Small entries not seen from the street.
- Locating the porch or entryway in a location obstructed by the garage or side of the house.
- Locating entryways and windows that are small and oriented to the interior or side of the site.



Homes with street-facing entries that are not hidden or deeply recessed, and good window placement for viewing the street are highly encouraged.



Home with a recessed entry: with line of sight from the front door limited by the living area wall and side of adjacent home, street activity is difficult to view.

G. Garage Frontage and Placement

Conventional suburban development typically places the garage in a prominent location on the lot closer to the street, with the house back farther from the street. The effects of garage-forward placement are to obstruct the view of the street from inside the house, to make the garage the most important feature of the house, to encourage the driver to enter the house through the garage door and prevent interaction with neighbors, and to decrease the appeal and safety of the street.

The following measures are suggested to minimize the visual impact of garages:

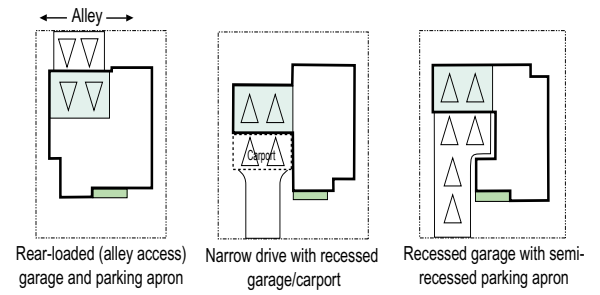
Encourage:

- For garages accessed from the street, the garage face should be recessed a minimum of five feet from the primary living area façade.
- Detached garages accessed from either an alley or a single-car driveway approach from the street.
- Alley-loaded designs particularly for narrow lots is strongly encouraged.

Discourage/Avoid:

- For garages accessed from the street, garage frontage comprising 50 percent or more of building frontage.

Desirable Garage/Parking Configurations



Undesirable Garage/Parking Configurations

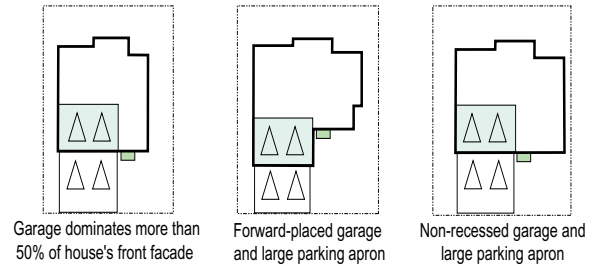


Fig. 6: Garage Frontage and Placement



Recessing the garage from the primary living area reduces its impact to the front facade while maintaining ample parking space.



Homes with garages that dominate the front of the home are not encouraged.

H. Driveways

Driveways can consume a substantial amount of lot area. A typical automobile is approximately seven feet wide and one to two feet of space is needed on either side to allow access to car doors. In order for the house to relate to the street and to allow observation of the street from inside the house, the width of the lot helps dictate the width of driveway access from the street (one- or two-car approach) or whether the garage should be accessed from an alley at the rear of the lot.

Encourage:

- Different paving treatment to driveways, including colored concrete, stamped concrete, paver-stone insets, etc.
- Single-car width driveways that widen to two-car aprons at recessed or detached garage.

I. Parking

Most transportation occurs through the use of the private automobile. Because of this, the Zoning Code requires a minimum of two parking spaces for every single-family house. With the increasing number of automobiles in every household, there is an increased need for parking, which is typically provided on the driveway and on the street. At the same time, the current trend is toward an increase in homeownership among single adults and the future will likely see an increase in the use of transit, walking, and bicycling for transportation, particularly as density increases and daily needs are better integrated into the fabric of the city.

Two enclosed off-street parking spaces will be required for each housing unit. Tandem parking spaces will not be allowed to meet this requirement. Tandem parking may be provided, in addition to the two off-street (side-by-side) parking spaces. In addition, one on-street parking space will be required for each dwelling. Off-street parking spaces located within 150 feet of the unit served, may also be considered.



Home with a single-car width driveway that widens to a two-car apron in front of recessed garage.



On-street parking in recessed spaces between street-tree wells alongside the right of way.



Off-street parking within garages and private parking aprons outside of garages, or community off-street spaces adjacent to homes.

J. Walls, Fences and Entry Features

Perimeter residences which are part of new developments should be oriented to existing streets, minimizing the extent of sound walls or rear yard walls, except where necessary due to acoustical requirements. Frontage roads are preferred in lieu of soundwalls wherever possible. The design of walls and fences, as well as the materials used, should be consistent with the overall development's design. Fence and wall color should be compatible with the development and adjacent properties.

Wall design and selection of materials should consider maintenance issues, especially graffiti removal and long-term maintenance.

Encourage:

- Soundwalls should have a rhythm rather than a single monotonous design along the entire length, and periodic entries to minimize driving and walking distances and integrate bike paths along the major roads.
- Landscaping and berms to minimize the visual impact of long continuous soundwalls.
- Additional landscape setbacks, street trees and accent trees at entries to improve the appearance of soundwalls.
- Concrete capstones on stucco walls to help prevent water damage from rainfall and moisture.
- Fences and/or walls visible from streets should be architecturally integrated with adjacent buildings as a means of visually tying buildings together.
- Low walls or fences (three- to four-foot high) at front or side yard patios where desired in lieu of porch railings, provided the wall/fence design is compatible with the architectural style of the house.
- Accent landscaping and trellises to set off development entries are desirable.

Discourage:

- Long walls separating subdivisions front street access and other subdivisions. This type of development restricts movement between neighborhoods and creates "dead" spaces along pedestrian corridors, as well as increasing driving and walking distances.
- Back-up and side-on conditions requiring walled streets.
- Wood fencing along streets since it is not a long-term quality material.



Encouraged: walls architecturally integrated with adjacent buildings; landscape buffer between wall and sidewalk/street.



Fence that has been constructed low but topped with lattice to create a sense of privacy without completely shutting away from street and sidewalk activity.



Discouraged: long walls that isolate neighborhoods from surrounding area; lack of landscaping that causes bare, paved-over appearance.

- Gated subdivisions. Gates create a “fortress” feeling and discourage interaction among neighborhoods and neighbors, prevent integration into existing neighborhoods, and discourage walking and bicycling for purposeful trips.

K. Landscaping

New small-lot single-family developments generally lack sufficient landscaping. The high lot coverage and minimal building separations create a harsher streetscape than houses on larger lots. Landscape design guidelines are intended to improve the appearance of the streetscape with landscaping and street trees to diminish the impact of the dense development and provide a softer appearance

Encourage:

- Street trees or yard trees regularly spaced at approximately 20’ to 25’ on center along each side of the street (minimum 1 per lot; refer to Title 10, Chapter 2, Modesto Municipal Code, for additional standards).
- Separated sidewalks with “tree lawns” (min. 4’ wide) (i.e. “parkways”). These may be planted in lawns or other appropriate ground cover (irrigation is required).
- 24-inch box tree specimens for all street and yard trees (consult the Urban Forestry Division, Parks, Recreation, and Neighborhoods Department, regarding tree selection).
- Tree species which create a continuous canopy at 15 years’ maturity.
- Accent trees at special locations within the neighborhood.
- Variety of planting palettes for front yard landscaping to soften the development, reinforce the home design, and add variety to the streetscape.
- Front yard landscaping which reinforces other design elements of the home such as vines on trellises, hedges or low fences and walls.



Landscaping should incorporate a broad palette of trees and plants that are native or compatible to the region’s climate, provide a full canopy at fifteen years of maturity, and add variety to the streetscape.

L. Mailboxes

Mailboxes should be located in highly visible, heavy use areas for convenience, to allow for casual social interaction, and to promote safety.

Encourage:

- Incorporate design features, such as a built frame, consistent with the development's architectural style.

Discourage:

- Pedestal-mounted cluster mailbox units.



Mailbox design should be consistent with architecture of homes.

M. Private Streets

Where private streets are used, they should incorporate design features such as special paving, neckdown intersections and separated sidewalks with street trees to indicate the change from public to private streets (Fig 7).

N. Street Design Elements and Access

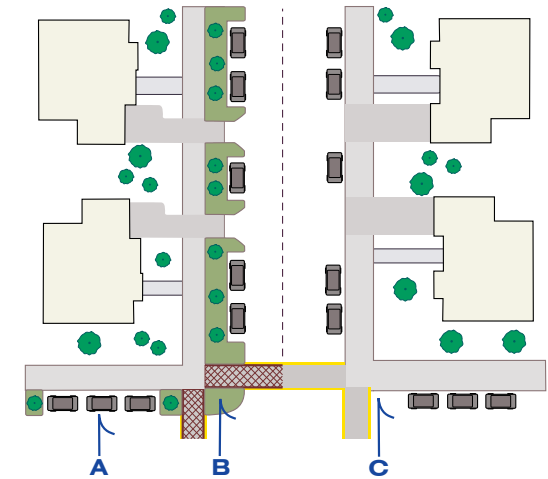
A street serves as more than a place to drive or park a car. Besides its most basic function as a transportation conduit for bicyclists and pedestrians, as well as for cars, a street serves architectural and social functions. Houses relate to the street on which they are located; streets serve to formalize the street edge and demarcate public and private space. Streets are where neighbors meet informally and neighborhood bonds are created. Minimal street connections within a subdivision and to the external street network increase the need to drive and the number of miles driven, discourage walking and bicycling, and reduce emergency access. The site shall be designed to provide accessibility for emergency vehicles.

Encourage:

- Residentially scaled street lights
- Separated sidewalks with street trees in planting strips or in tree wells; sidewalks shall be designed in compliance with ADA requirements.
- Accent paving at neighborhood entries and at crosswalks
- Multiple ingress and egress points into subdivisions, which allow more even dispersal of traffic through a neighborhood, decrease vehicle miles driven, and increase the ability to walk or bicycle for short trips.
- Blocks less than 600 feet long.
- Incorporate design features, such as a built frame, consistent with the development's architectural style.



Homes that face the street as well as sidewalks separated from the street by planting strips and tree wells are strongly encouraged.



Strongly Encouraged: parking recessed from street, sidewalk separated from street, "neckdown" intersections with enhanced paving at crosswalks (Exhibit B).

Discouraged: Sidewalks adjacent to streets, roll-over curbs. (Exhibit C)

Acceptable parking variation: recessed from street while adjacent to sidewalks, with landscaped tree-wells at regular intervals (Exhibit A).

Fig. 7: Sidewalk and On-Street Parking Configurations (Private Street)

- “Neckdown” streets at intersections to facilitate pedestrian safety, enhance landscaping and reduce excessive speeding, subject to review and acceptance by the Public Works Department and Fire Department.

Discourage/Avoid:

- “Cobra head” street lights (see Section P, Lighting)
- Rollover curbs
- Sidewalks adjacent to street
- Large-radius corner
- Cul-de-sac and dead-end streets

O. Alley Design

Alleys are desirable because they eliminate the impact of the garage door and driveway apron on the streetscape and eliminate driveway access conflicts on streets with higher traffic volumes or speeds. Alleys also allow homes to front lot, parks, or open space without a road separating the homes from such features. Additionally, alleys provide a convenient placement for the unattractive activities of the City, such as garbage collection and utility maintenance, improve emergency access by creating two points of entry or access to each lot, and create a more walkable street while increasing the residents’ ability to survey activity on the street from inside the house.

Encourage:

- Alleys should be configured so that activity in the alley can be easily observed from a single point.
- Special accent paving at entries.
- Landscaping should be consistent with the development, with four-foot landscape strips and a minimum of one tree per lot.
- Protection of trees in the form of tree guards or substantial double-staking to protect trees placed in the four-foot landscape planting space. Attention should be directed to proper selection of tree species and forms that can prosper in these more limited conditions.
- Building or pedestal lighting should be provided from each lot.

Discourage/Avoid:

- Dead-end alleys. If dead-end alleys are necessary, they should be no more than 100 feet long.



Rollover curbs are strongly discouraged: drivers not inhibited by square-cut curbs from moving or parking on the sidewalk can pose serious danger to pedestrians.



Example of alley-accessed garages and the eliminated impact of driveways and garages from the front of the home and streetscape.

P. Lighting

Lighting should relate to the pedestrian scale of residential neighborhoods and should be considered a design element, rather than simply utilitarian.

Encourage:

- Light standards less than 15 feet in height. Decorative Visco VI-X-1-OF standard or equivalent standard with the same bulb type as the Visco are encouraged.
- Bollard lighting is encouraged along walkways.
- Metal halide luminaries.

Discourage/Avoid:

- Overhanging "cobra head" light fixtures.

Q. Homeowners' Associations

A Homeowners' Association (HOA) is required to provide ongoing maintenance for any and all of the following project elements:

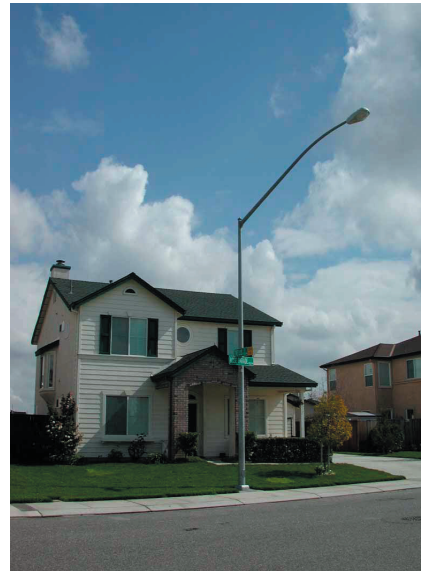
- Private streets
- Private utilities
- Alleys
- Private common open space including recreation facilities (not maintained by a community facilities district (CFD)).
- Stormwater drainage basins, and related facilities (catch basins, swales, etc.), (not maintained by a CFD).
- Common area landscaping and lighting
- All other common areas, utilities, and facilities

R. Utilities, Infrastructure & Easements

Any and all private infrastructure shall be constructed to City standards. Public utility easements shall be provided for all public utility connections, in compliance with City Standards.



Pedestrian-scaled decorative light fixtures add to the intimacy and character of a neighborhood.



Strongly discouraged: oversized utilitarian fixtures such as this "cobra-head" example, which outscapes the surrounding homes and lends little character to the neighborhood.

II. COURTHOMES

INTENT

Courthomes are a type of housing where houses on individual small lots are arranged around a common driveway. These present a special challenge in site planning due to the tight arrangement of lots and the arrangement of houses on the court, as well as the shared driveway, which requires that cars be parked inside the garage in order to allow the residents driveway access and egress.

The following guidelines apply specifically to courthomes and will be considered in addition to the previous small-lot guidelines contained in Section I. When evaluating courthomes, where there is any apparent conflict, the guidance below supercedes that above.

A. Setbacks and Separations

Similar to setbacks in conventional development, setbacks and separations in courthomes are used to create access around a building, provide adequate space for utilitarian functions, allow the passage of light and air between buildings and create open space, while minimizing noise and intrusion and maximizing privacy. The following provisions are intended to serve those purposes.

The following guidance establishes siting and setback criteria that supplement Section I.

The minimum front setback for courthome units adjacent to streets should be:

Table 4: FRONT SETBACKS (IN FEET)	
Living Area	12
Porches (at least six feet deep)	10
Attached Garage (entry side)	Strongly Discouraged (a)
Attached Garage (non-entry side)	12

a) such design (garage entry facing street, not accessed by private court) may only be considered for exceptional circumstances.



An example of courthomes and their private, common drive.



Setbacks should not be so narrow that access around buildings is difficult, and passage of sunlight is limited.

Courthome separations and perimeter setbacks are to be provided as follows:

- The minimum separation between building faces of units on one court and those on another court should be 14 feet (Fig. 9).
- Separations between adjacent buildings in the same court should be at least ten feet.
- For privacy, second-floor windows, except for clerestory windows, should be avoided on elevations which overlook private open space areas of adjacent units. In instances where second-floor windows are unavoidable, they should be setback at least 15 feet from property lines and separated from adjacent open space areas by tall shrubs or trees.
- On corner sides, conform with the “clear vision triangle” area requirement at street intersections per City Standards, Detail No. 711.
- On sides that back up to a street, all buildings should be set back 15 feet from the property line.
- Minor architectural projections, such as fireplaces and bay windows, may project into setback or separation by up to two feet for a length not to exceed ten feet or 20 percent of the building elevation length, with minimum three-foot clearance.

Encourage:

- Houses adjacent to a local residential street should face the street.
- All courthomes should take garage access from the court.

B. Courtyard Design

Courthomes with six units accessed from a single drive (courtyard) present site development issues and concerns. With respect to courthomes with drives serving six units, the ends of the courtyard visible from the adjoining street are dominated by garage doors. Typically, these units are the same model type and offer little or no architectural variation from one another.

Other issues surface with potential vehicular conflicts with internal units at the ends of the court. The following design provisions are recommended for courthome developments.

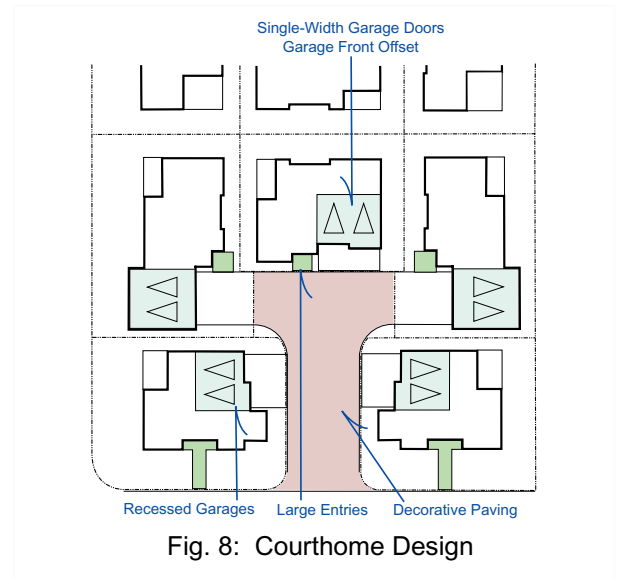


Fig. 8: Courthome Design



Homes adjacent to street should face the street with garage access from the courtyard.



Garages should be recessed behind the dwelling unit.

Encourage:

- The maximum number of units accessed from a single courtyard should be limited to five. Sites with unusual configurations may include an occasional courtyard which serves up to six units.
- Courtyards should not exceed 100 feet in length (Fig. 9).
- The paved circulation portion of the courtyard should have a minimum width of 16 feet, but larger dimensions are encouraged to foster the “courtyard” image. The entrance to the courtyard, at the street, should be at least 20 feet in width and 100 feet in depth (measured from the face of curb) (Fig. 9).
- Recess garages behind the main dwelling unit similar to typical lots, to minimize the visual impact of the garage door and parking apron. Use roll-up doors for garages.
- To discourage obstruction of the driveway, garage aprons should be either less than six feet deep or more than 18 feet deep.
- Parking aprons should be limited to the rear units not visible from the street.
- Paved areas in front of garage doors should have a minimum back out dimension of 26 feet.
- Trees and large landscape fingers between parking aprons are strongly encouraged to break up the expanse of paving and view of garages. An average of 200 square feet of landscaping per unit should be provided within the courtyard. Trees should be provided at the rate of one front yard tree at each interior lot minimum. No part of the access street setback area may be counted toward the 200-square-foot requirement.
- Decorative paving should be provided in the courtyard (Fig. 9).
- Green space should be placed at the end of court as a focal point.
- Variation of building types and elevations on end units is encouraged.

Discourage/Avoid:

- Terminating vistas from the street should not be garage dominated.
- In order to minimize paving within the courtyard, parking aprons accommodating parking in front of the garage are discouraged in the front of the “front” unit garages; parking aprons should be limited to the rear units.

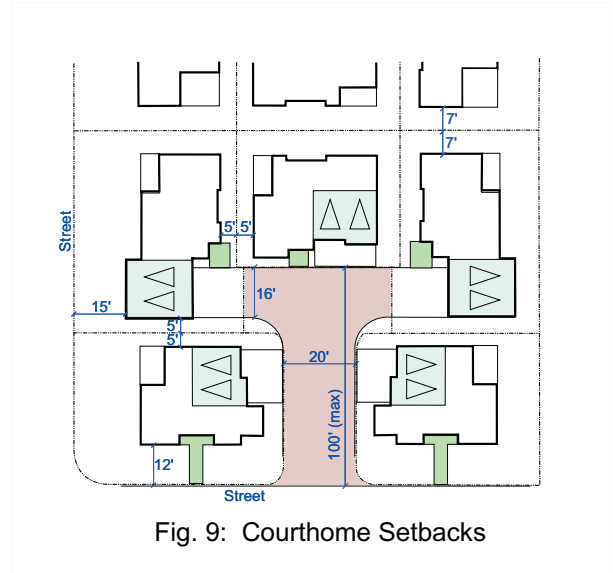


Fig. 9: Courthome Setbacks



Encouraged: decorative paving accent on private drive, such as the paver stones shown in above photo.



Discouraged courtyard entry and house placement: plain asphalt or concrete and garages that visually dominate end of drive.

- Asphalt concrete paving should be avoided in the courtyard.

C. General Building and Open Space Design

Building design is a particularly important component of the courthome concept. Courtyards should be sharply defined by the near continuous faces of the buildings clustered around the perimeter. The following design provisions supplement those identified in Section I.

Encourage:

- All units in a court should share a common architectural theme which visually links them.
- Enlarge entries with porches to accent corners and interior vista.
- 300 square feet of common open space per unit should be provided for projects with more than 15 units that are not adjacent to a public park (refer to common open space guidelines on page 3).
- Ten-foot minimum dimension for private open space (rear yard area)

D. Landscape Maintenance Requirements

A Homeowners Association must be established to maintain front yard, courtyard area and common area landscaping within the project.



Homes should have a similar architectural theme to visually tie the neighborhood, but also feature a variety of building materials and color palettes to avoid a “cookie-cutter” look to the development.



Spacious common areas are strongly encouraged. Also encouraged are interior courts, paseos, and large landscaped areas at private drive entries.